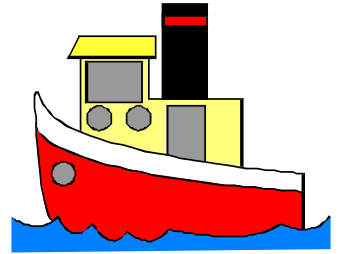


Wheels and Floats



Newsletter November 2016

TAURANGA MODEL MARINE AND ENGINEERING CLUB INC.

The Secretary
PO Box 15589
Tauranga 3112
Palmerville Station Phone 578 7293

Miniature Railway Memorial Park
Open to Public, weather permitting
Sundays in Summer: 10am to 4pm approximately
Website: www.tmmecc.org.nz

MEETINGS

General Members Meeting every first Tuesday 7pm.
Committee Meeting every second Thursday at 7pm.
Maintenance Tuesday mornings from 9am.
Engineering discussions Tuesday evenings 7.30pm.

COMMITTEE

President:	Peter Jones	543 2528
Secretary:	Bruce Harvey	548 0804
Co-Treasurers:	Owen Bennett	544 9807
	Clive Goodley	572 2959
Committee:	Warren Belk, Shane Marshall, John Stent, Bruce McKerras, Peter Lindsay, John Nicol, Mike Webber	
Boiler Committee:	Peter Jones, Paul Newton, Bob Batchelor, Bruce McKerras, John Heald	
Safety Committee:	Warren Karlsson, Bruce Harvey, John Nicol, Malcolm George	

EDITOR: Roy Robinson 5491182
royrobkk@gmail.com

CONVENERS

Workshop:	Malcolm George, John Nicol
Track :	Bruce Harvey, John Stent, Russell Prout
Marine:	Warren Belk
Librarian:	John Nicol
Rolling Stock:	Clive Goodley, Mike Webber
Website:	Murray de Lues
Driver Training:	Clive Goodley, Mike Webber
Club Captain:	Bruce McKerras

OPERATORS 2016

7 November	Ron Salisbury
12 November	G Barns
13 November	B Fitzpatrick
20 November	C Goodley
27 November	B Harvey
4 December	P Jones
11 December	W Karlsson
18 December	P Lindsay
25th Dec 2016 & 1st Jan 2017	Closed
8th January	B McKerras
15th January	R Salisbury

Trust Power Christmas Parade Float

The Trust Power Tauranga Christmas Parade is being held on Saturday the 3rd December. We need as many members as possible to turn up at our club rooms on the day of the parade by 8 o'clock am to help with final preparations in decorating the floats and to accompany our floats during the parade. Please wear your club shirts and caps if you have them. Add a bit of Christmas cheer to your ensemble including headgear. The theme is the same as last year "Santa's Engineering Workshop". Bring any Christmas decorations to decorate our floats with you. For those who can help with preliminary setting up of the floats there is a working bee at Russell Prout's residence at Omokoroa on Saturday 26th Nov starting 9 o'clock. Please let Shane Marshall know if you can attend on the 26th. Otherwise see you all early on the 3rd December

Your enthusiastic support is needed so we can replicate last years win!"

Shane Marshall

Presidents Report

Greetings members.

By the time this goes to press our open weekend will have come and gone, at this stage signs are partly wet but can't do much about that. A rail safety program initiated by Shane is to happen on Sunday targeting children and rail crossings as part of our campaign to support rail safety week next year.

We have a few busy weeks ahead of us, firstly the Christmas Parade on Saturday the 3rd of December, members indicated strong support for this event so get in touch with Shane and ask how you can help.

Next up is the Club Christmas get together on Saturday the 10th December and once again Bruce Harvey has kindly agreed to act as host at his home in Youngson Road Omokoroa, please bring a contribution of salad or dessert our club will provide the meat.

The next event will be the National Trust Power Volunteers awards in Rotorua on a date yet to be advised. Our club has enlisted some professional people to assist with our presentation. Always plenty to do.

A notice of motion was presented to our committee moved by Trevor Chapman and seconded by Bruce Harvey that Malcolm George be made a life member in recognition of the work Malcolm has done for our club over a long period of years, both around the track and as a committee member. Our next club night on the 6th December will be a special general meeting to vote on this notice of motion. Please attend and vote for Malcolm.

Regards

Peter Jones.



Diesel powered carriage Pichi Richi Railway (see article pg10)

AALS Annual Australian Convention 2017

Where : The Convention will be held at Newcastle NSW The hosting club is the lake Macquarie Steam Locomotive Society at Edgeworth.

When : Easter 2017 : Thursday 13 April to Monday 17th April 2017

Contact : Your Editor or President for more information should you be interested.

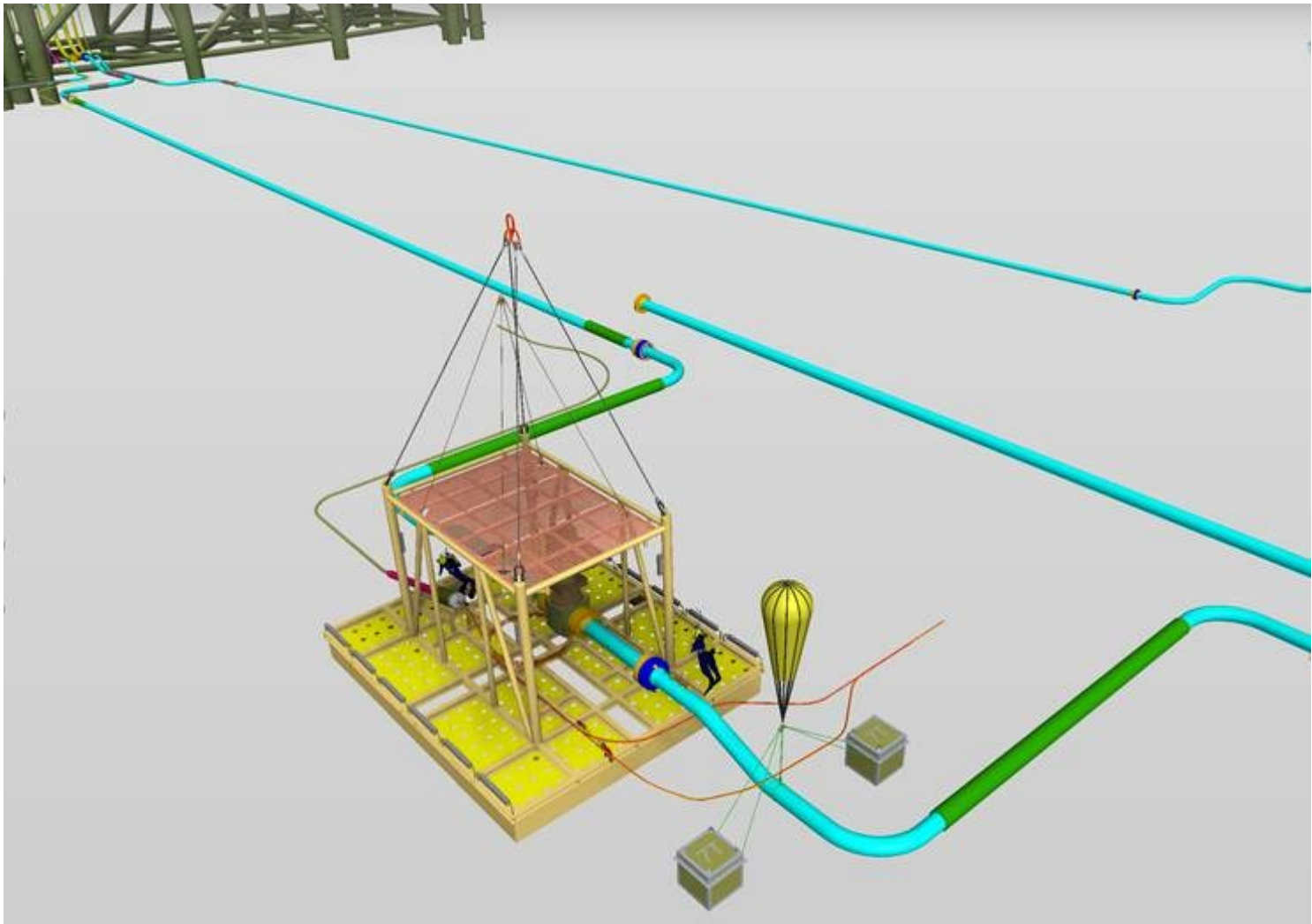
HESS Project Execution

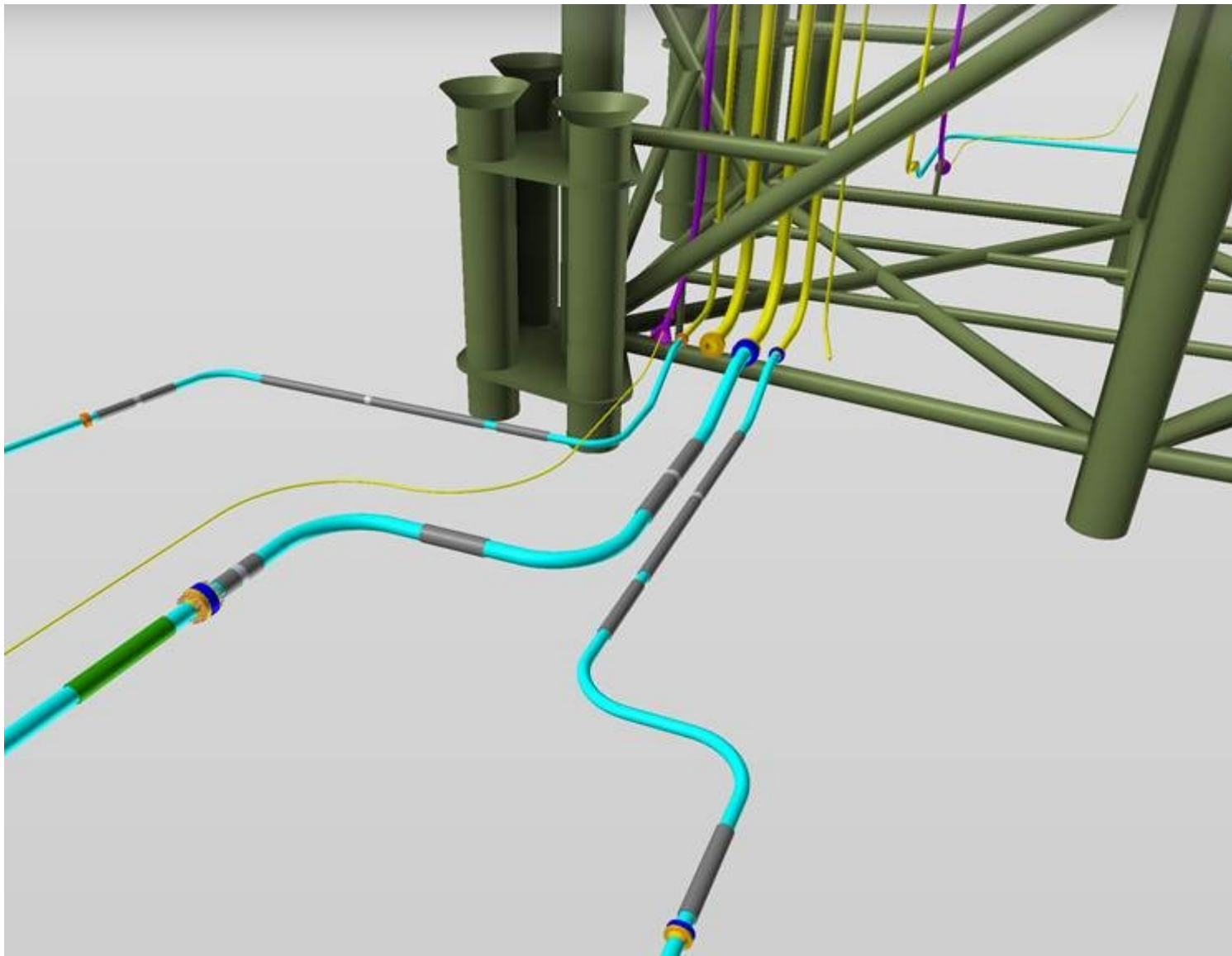
In August 2016 Kreuz Subsea completed the preparation works, mobilisation and departure of the DSV Kreuz Installer (K.I.) to the North Malay Basin Full Field Development for HESS.

On September 1st the DSV K.I. arrived on location after rendezvousing with two materials barges to commence the installation works.

Four SSIV's, Sixteen interconnecting spools and a shipping container load of stud-bolts, some up to 88mm diameter (3.5 inch) and 1250mm long and weighing in at around 50kg are to be installed, but first a series of carefully executed activities need to be carried out.

Pipelines, pre-laid (by others in 2015) were fitted with temporary connecting spools (up to 30m in length) that enable the pipe-lay barge to deploy pipe on a continuous basis from one end (near the platform) to the other up to 20km away at a Remote Wellhead Platform (WHP) or Pipeline End Termination Skid (PLTS). These temporary spools are to be removed by the DSV for the installation and hook up of the Sub-sea Isolation Valve Skids (SSIV). The SSIV's can be installed whilst the spools are in place as they are usually offset from the pipeline so as to incorporate an expansion loop thus reducing the pipe stress at that location due to variation in pipeline temperatures and resultant thermal growth.





Closing spools installed between Platform and Pipelines

To install the closing spools (in the shape of an 'S') first requires a number of subsea measurements before spool fabrication can commence. These measurements make use of a set of Metrology Jigs set up by divers to correctly check the angle of flanges and actual distances between them (keeping in mind the flanges are now offset by more than 20m). Vertical angles are also measured so that all three dimensions (X, Y & Z) of the closing spool are confirmed.

Metrology Jigs are used in pairs where one unit is set up on each of the two flanges where a closing spool is to be installed and a taught wire (fine wire tightened by hand winches from the jig). The wire forms the direct line between jigs and shows a resultant angle between the line of pipe and the direct line between the flanges. Once all the relevant details are recorded the jigs are repositioned at opposite ends and the process repeated to confirm these dimensions.

On the DSV, engineers are required to interpret the dimensions and angles and transfer this information into the pipe spool fabrication drawing. This drawing is then used by the fitters, riggers and welders on deck to assemble the pieces, cut, prepare and weld (minimum of three locations (for X, Y & Z) to complete the assembly of each closing spool.



Several pipe spools can be seen on the deck of the DSV at various stages of fabrication, field joint coating, preparation for hydrostatic testing and rigged ready for deployment

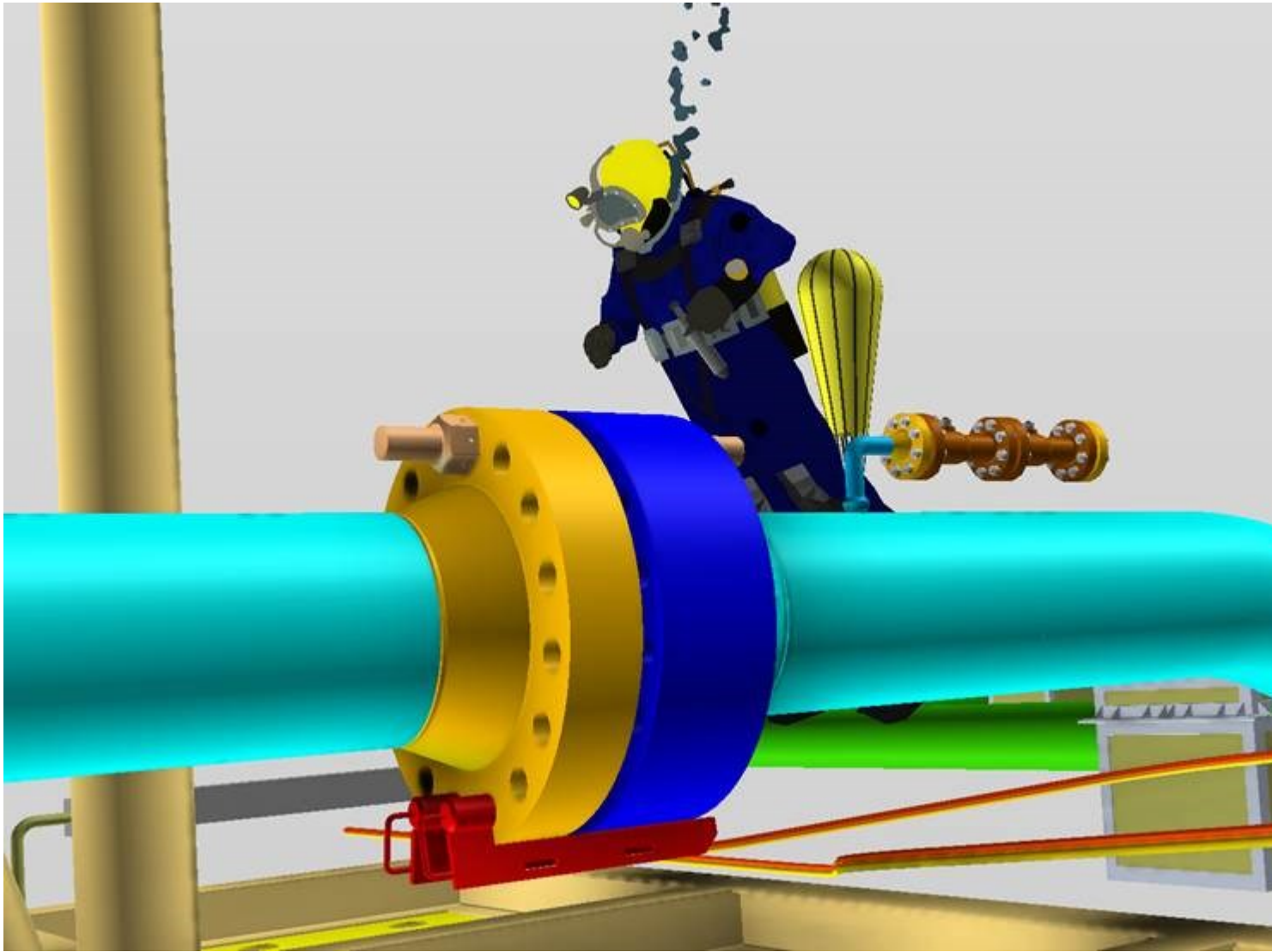
All welds must cool down from the welding process before undergoing NDT (non-destructive testing) to verify the integrity of the weld. When NDT is complete and passed by the inspector each of the weld joints (known as field joints) shall be cleaned thoroughly by grit blasting before being coated and wrapped with materials to protect the bare metal from corrosion.

When completed on deck the closing spool is then rigged for lifting by the DSV crane (to a near level attitude) and then lowered to the installation location where it must be aligned and bolted in place.



Spool entering the water with 4-part lifting bridle and levelling rigging. The yellow objects are air lift bags used to reduce the weight of the spool in water during final positioning and fit up

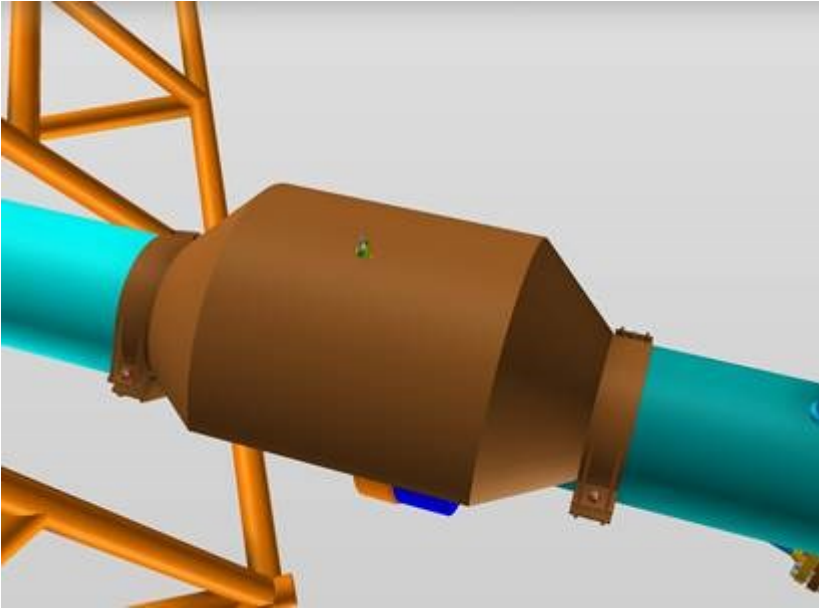
Alignment of flanges is achieved by using Flange Catchers (support and guiding jigs fitted to the flange adjacent to the tie-in location). The spool is then lowered onto these flange catchers and stud-bolts (20 per flange) are inserted to eventually secure the flange joints. Before closing the flanges, chemical treatment (sticks) are inserted to remove oxygen from the water inside and inhibit corrosion. A gasket (to seal the flange joint) is also inserted between each pair of flanges and once aligned the flange joint is tightened, firstly by hand and then using hydraulic tensioning equipment.



Flange catchers (shown in red below flange set) positioned by divers to assist with flange alignment, then bolts inserted

When the subsea assembly of each flange set is completed the special gasket (KaMos) used in the joint is hydrostatically tested (high pressure water) to verify the seal. The joint is deemed to be completed when this gasket test hold pressure. Once the flanges at each end of a closing spool are fully assembled and gaskets tested the flanges are then covered using flange protectors.

The completed flange assembly now has stud-bolts protruding from each side and the pipelines are often in waters where fishermen drag large nets so a purpose built protective cover over each flange (called a flange protector) is installed. This unit guides the fish nets up and over all the protruding stud-bolts to prevent them being caught up.



Flange protector shown surrounding the assembled flanges

A complete ROV survey of the spool is then carried out and this spool installation is then completed. Time then to move on to another and repeat the process again until all 16 spools are installed and ready for pipeline testing and pre-commissioning.

Russell Prout





Oliver Duncan with his Tamar Lego model (Has the boiler got a certificate?)



Pichi Richi Rail Trip

We arrived in Adelaide just after one of the worst storms in the State's history. The whole State had no power for 24 hours which has created an ongoing political storm!!! On the drive from Adelaide to Port Augusta we saw evidence of flooding waterways and I believe we were one of the last vehicles to get through that road. We had booked into accommodation at the 1878 Hotel Flinders which turned out to be an interesting and pleasurable experience mainly centred around the bar. We fronted up to the Port Augusta Rail Station next morning to find The Ghan at the Mainline Standard Gauge platform. Subsequently we found out the link to Adelaide had been closed by floodwaters and 300 odd passengers had been bussed to Adelaide and the "to be" passengers bussed back to Port Augusta. Now Port Augusta Rail Station isn't exactly "Kings Cross" so only the double head engines, the generator wagon and 2 carriages were against the platform the other 38 carriages were way back and beyond.....!!!! I have no idea how they turned at least the loco's around but 24 hours later it was gone back north.

The Puchi Richi narrow gauge rail is on the other side of the platform and the W22 loco was soon being connected to the 5 carriages. It was all aboard for the 3 ½ hour trip to Quorn some 75 k odd away. The line rises from sea level to the summit at 1332 feet before dropping to 961 feet at Quorn. At Woolshed Flat approximately half way a second engine and 5 carriages which had brought other passengers from Quorn down was doubled headed and we chugged up what was the steepest section to the Summit and on down to Quorn. This is a typical meandering track up through the Flinders Ranges continually crossing the road link and streams between the two towns. The real excitement comes to a head when the train



crosses a viaduct which is shorter than the TMMEC one but is quite high. The carriages are all period restored and with open sides provide great viewing. There wasn't a huge crowd and we had room to spread out as we only had four others plus a conductor in our carriage. The conductor gave a great running commentary which was helpful for taking photos. On the return trip we saw an area obviously set up for a wedding and as we got closer to Port Augusta we stopped to allow the group who had hired a diesel powered carriage connected to a standard carriage with all the wedding party and guests heading up to the site for the ceremony. What a great occasion they would have had with no drink driving!!!!

Thoroughly recommend the trip and pick a day when the double header runs.

Roy Robinson



Puchi Richi Railway



Common Sense

An Absolutely Brilliant Obituary Printed In The London Times Thursday, 7 April 2016, 1:33PM

Today we mourn the passing of a beloved old friend, Common Sense, who has been with us for many years.

No one knows for sure how old he was, since his birth records were long ago lost in bureaucratic red tape.

He will be remembered as having cultivated such valuable lessons as knowing when to come in out of the rain, why the early bird gets the worm, life isn't always fair and maybe it was my fault.

Common Sense lived by simple, sound financial policies (don't spend more than you can earn) and reliable strategies (adults, not children, are in charge).

His health began to deteriorate rapidly when well-intentioned but overbearing regulations were set in place.

Reports of a 6-year-old boy charged with sexual harassment for kissing a classmate, teens suspended from school for using mouthwash after lunch and a teacher fired for reprimanding an unruly student only worsened his condition.

Common Sense lost ground when parents attacked teachers for doing the job that they themselves had failed to do in disciplining their unruly children.

It declined even further when schools were required to get parental consent to administer sun lotion or an aspirin to a student, but could not inform parents when a student became pregnant and wanted to have an abortion.

Common Sense suffered further as the churches became businesses and criminals received better treatment than their victims.

Common Sense took a beating when you couldn't defend yourself from a burglar in your own home and the burglar could sue you for assault.

Common Sense finally gave up the will to live after a woman failed to realise that a steaming cup of coffee was hot. She spilled a little in her lap, and was promptly awarded a huge settlement.

Common Sense was preceded in death by his parents, Truth and Trust, by his wife, Discretion, by his daughter, Responsibility, and by his son, Reason.

He is survived by his 5 stepbrothers; I Know My Rights, I Want It Now, Someone Else Is To Blame, I'm A Victim, and Pay Me For Doing Nothing.

Not many attended his funeral because so few realized he was gone.

If you still remember him, pass this on. If not, join the majority and do nothing.